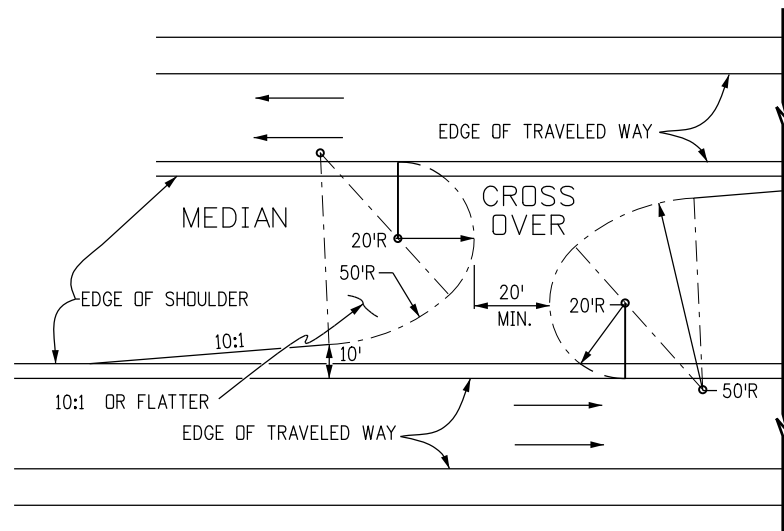


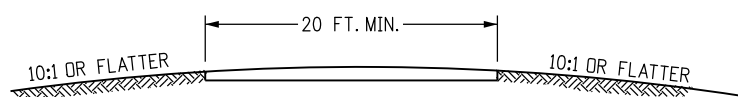
MEDIAN WIDTH LESS THAN 50 FT.



MEDIAN WIDTH GREATER THAN 50 FT.

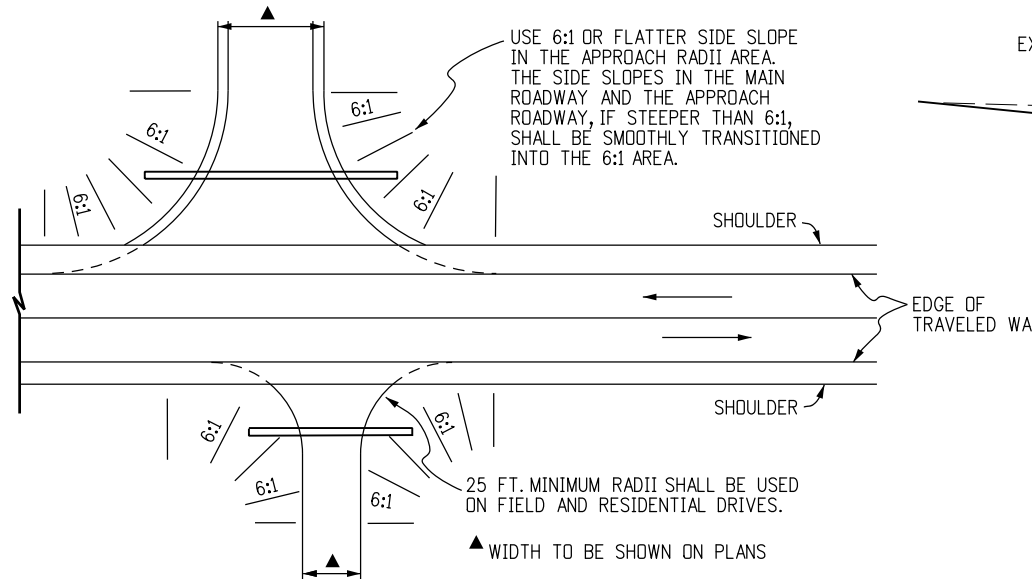
TYPICAL PLANS FOR EMERGENCY MEDIAN CROSS OVER

LOCATION OF RADIUS POINTS MAY BE ADJUSTED FOR BEST FIT



TYPICAL SECTION FOR MEDIAN CROSS OVER

ANY REQUIRED PIPE OR INLET FOR MEDIAN DRAINAGE SHALL HAVE A TRAVERSABLE DESIGN AS SPECIFIED ON THE PLANS



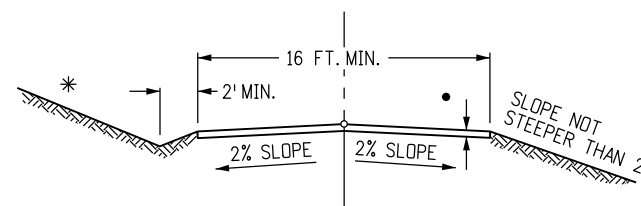
USE 6:1 OR FLATTER SIDE SLOPE IN THE APPROACH RADII AREA. THE SIDE SLOPES IN THE MAIN ROADWAY AND THE APPROACH ROADWAY, IF STEEPER THAN 6:1, SHALL BE SMOOTHLY TRANSITIONED INTO THE 6:1 AREA.

25 FT. MINIMUM RADII SHALL BE USED ON FIELD AND RESIDENTIAL DRIVES.

▲ WIDTH TO BE SHOWN ON PLANS

TYPICAL PLANS FOR SIDE APPROACH ROAD

SIDE DRAINS SHALL BE LOCATED BEYOND THE CLEAR ZONE, OR WHEN WITHIN THE CLEAR ZONE, THEY SHALL BE INSTALLED WITH END SECTIONS CONFORMING TO A 6:1 SLOPE. FIFTY FT. RADII SHALL BE USED ON INTERSECTING ROADS, EXCEPT FOR FIELD AND RESIDENTIAL DRIVES OR UNLESS OTHERWISE SPECIFIED ON PLANS. RADII MAY BE VARIED TO SUIT FIELD CONDITIONS.

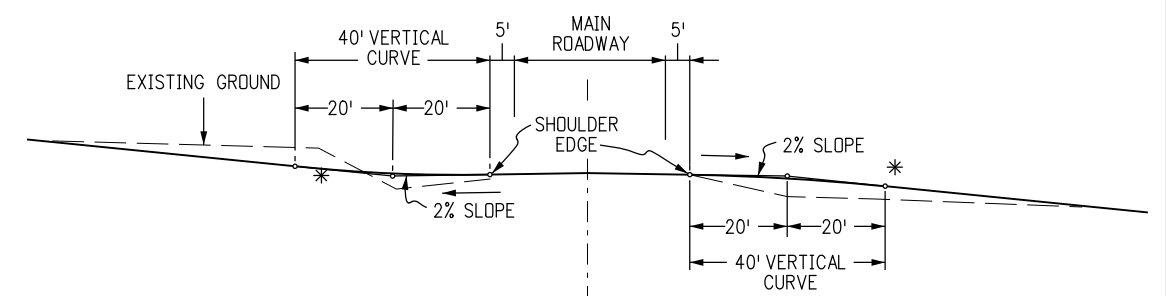


1/2 CUT SECTION 1/2 FILL SECTION

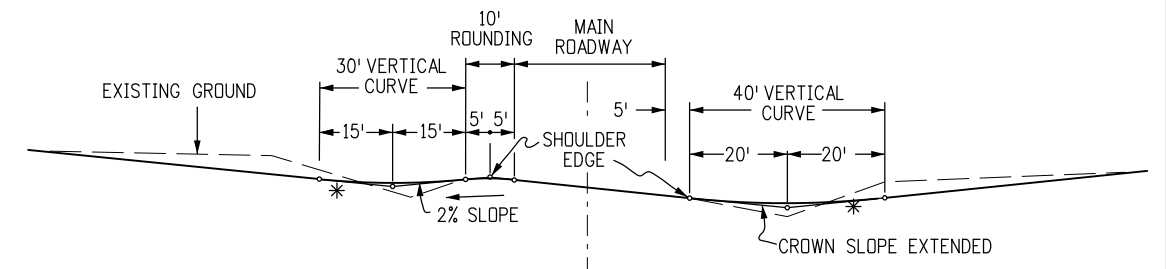
TYPICAL SECTION FOR APPROACH (ACCESS) ROAD

NOTE: ROAD APPROACHES WHICH REQUIRE HMA (ASPHALT) PAVEMENT SHALL BE PLACED AT THE FOLLOWING DISTANCES BACK FROM THE ROADWAY EDGE OF PAVEMENT:

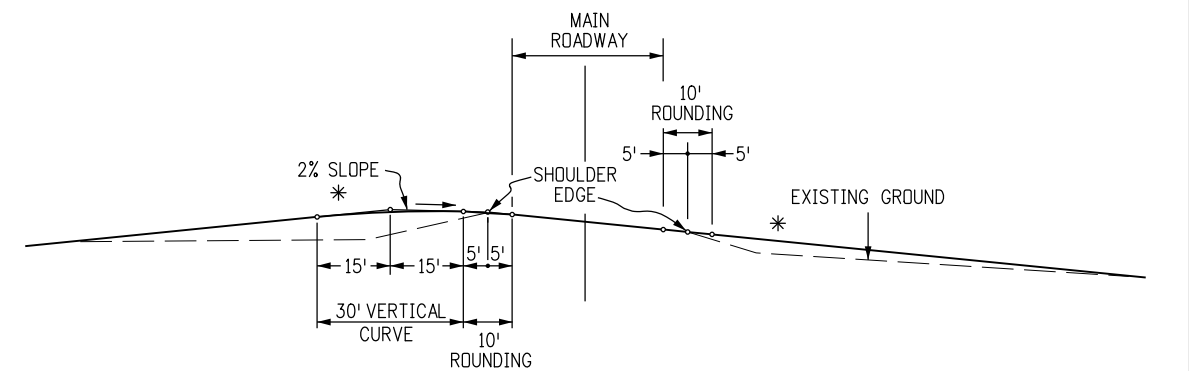
1. RESIDENTIAL OR AGRICULTURAL FIELD ENTRANCES - PAVE 4 FEET BACK.
2. THREE OR MORE RESIDENCES OR COMMERCIAL PROPERTY - PAVE 20 FEET BACK OR TO ROW LINE, WHICHEVER IS LESS.
3. PUBLIC STREET - PAVE 50 FEET BACK OR TO ROW LINE, WHICHEVER IS LESS.
4. IF EXISTING ACCESS IS PAVED, THEN FEATHER NEW ASPHALT OVERLAY A MINIMUM OF 2 FEET BACK OR AS DIRECTED BY THE ENGINEER.



1/2 CUT SECTION 1/2 FILL SECTION
STANDARD CROWNED SECTION



SUPERELEVATED CUT SECTION



SUPERELEVATED FILL SECTION

VERTICAL ALIGNMENT SIDE APPROACH ROADS INTERSECTING MAIN ROADWAY

* TANGENT SLOPE NOT STEEPER THAN 8% BEYOND THE VERTICAL CURVE. THE SLOPE MAY BE STEEPER, IF REQUIRED, TO MEET EXISTING APPROACH SLOPE. HOWEVER, APPROACH ROAD SLOPE SHOULD NOT BE STEEPER THAN EXISTING SLOPE.

Computer File Information

Creation Date: 07/04/12	Initials: DD
Last Modification Date: 07/08/13	Initials: LTA
Full Path: www.coloradodot.info/business/designsupport	
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CAD Ver.: MicroStation V8	Scale: Not to Scale Units: English

Sheet Revisions

Date:	Comments
07/08/13	Added notes to Approach Road Typ. Sec. detail.

Colorado Department of Transportation

4201 East Arkansas Avenue
Denver, Colorado 80222
Phone: (303) 757-9083
Fax: (303) 757-9820

Project Development Branch DD/LTA

APPROACH ROADS

Issued By: Project Development Branch July 4, 2012

STANDARD PLAN NO.

M-203-1

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