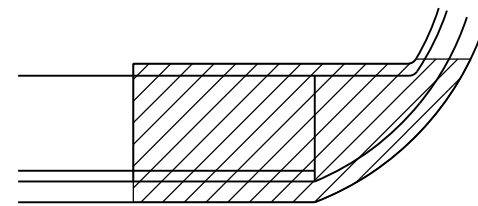
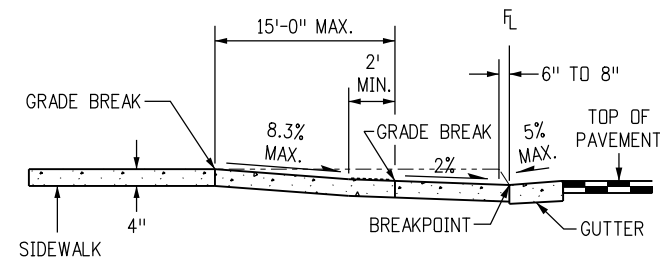
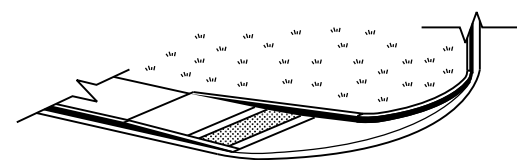
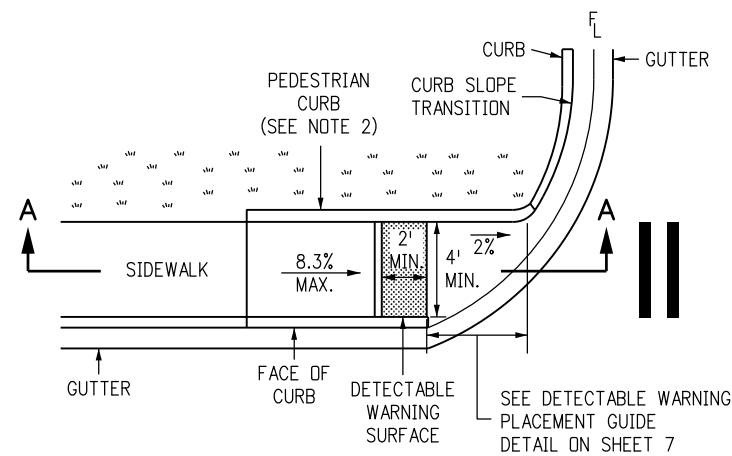
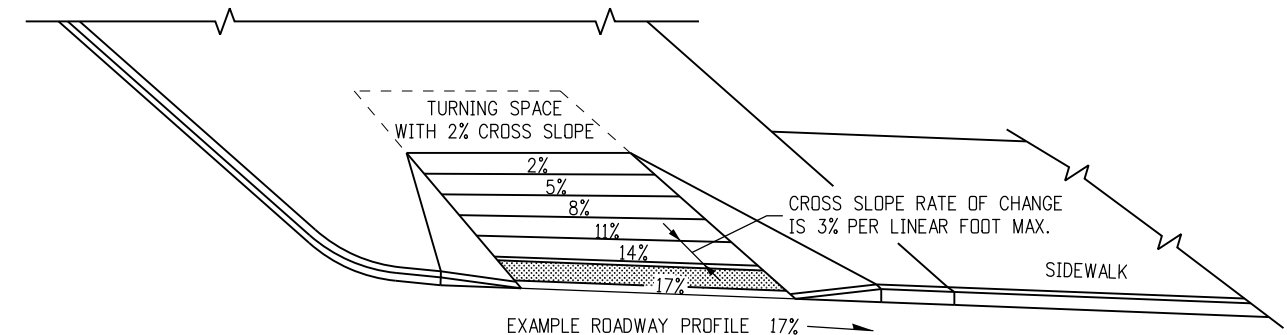


NOTES

1. RAMP GRADE BREAKS SHALL BE PERPENDICULAR TO THE RUNNING SLOPE.
2. PEDESTRIAN CURB MAY BE OMITTED IF THE GROUND SURFACE AT THE BACK OF THE CURB RAMP AND TURNING SPACE WILL BE AT THE SAME ELEVATION AS THE CURB RAMP AND TURNING SPACE, OR THERE WILL NOT BE ANY MATERIAL TO RETAIN.

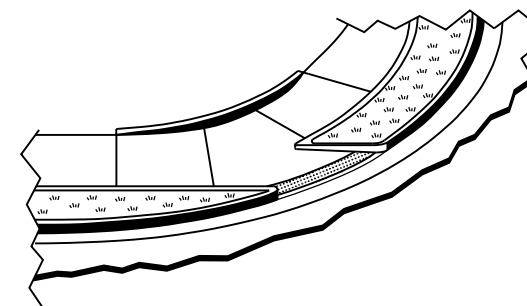
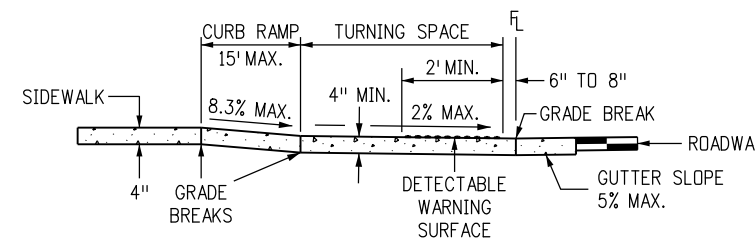
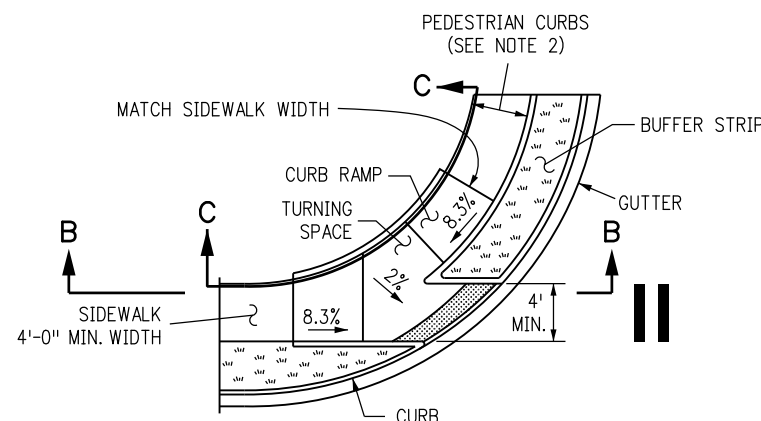


CURB RAMP TYPE 4A

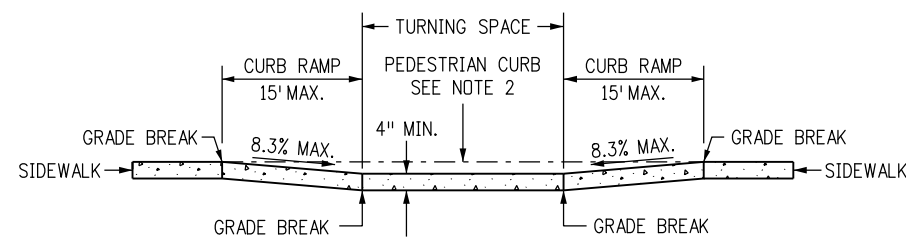


RAMP CROSS SLOPE TRANSITION TO MATCH ROADWAY PROFILE (APPLICABLE TO ALL CURB RAMP TYPES)

- CURB RAMP CROSS SLOPES AND TURNING SPACES SHALL BE:
- A. 2% MAX. WHEN A YIELD OR STOP CONTROL IS PRESENT.
 - B. PERMITTED TO EQUAL THE ROADWAY GRADE WHEN THERE IS NO YIELD OR STOP CONTROL, WHEN A TRAFFIC SIGNAL IS PRESENT, OR AT A MIDBLOCK CROSSING LOCATION.



RAMP PAY AREA



CURB RAMP TYPE 4B

FOR USE WHERE PEDESTRIAN CROSSING IS IN ONE DIRECTION ONLY.

Computer File Information

Creation Date: 12/01/16	Initials: JBK
Last Modification Date: 02/23/17	Initials: LTA
Full Path: www.coloradodot.info/business/designsupport	
Drawing File Name: 6080104010.dgn	
CAD Ver.: MicroStation V8	Scale: Not to Scale Units: English

Sheet Revisions

Date:	Comments
02/23/17	Added and revised the Curb Ramp details and General Notes.

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CURB RAMPS

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STANDARD PLAN NO.

M-608-1

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