MEDIAN 10:1 OR FLATTER CROSS OVER R=½W W=MEDIAN WIDTH

30'

EDGE OF SHOULDER

EDGE OF TRAVELED WAY

MEDIAN WIDTH LESS THAN 50 FT.

MEDIAN

CROSS OVER

EDGE OF TRAVELED WAY

20'R

50'R

20' MIN.

20'R

EDGE OF SHOULDER

10:1

10'

10:1 OR FLATTER

50'R

EDGE OF TRAVELED WAY
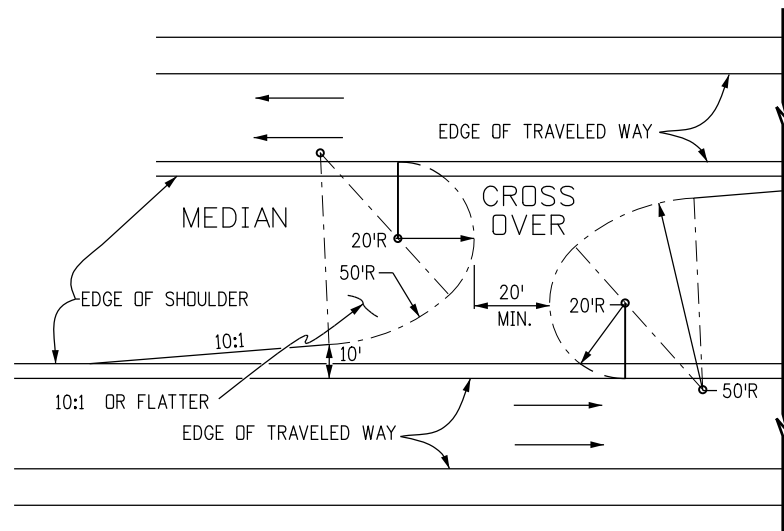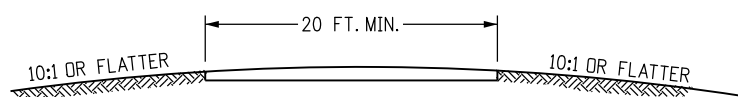
MEDIAN WIDTH GREATER THAN 50 FT.

TYPICAL PLANS FOR EMERGENCY MEDIAN CROSS OVER

LOCATION OF RADIUS POINTS MAY BE ADJUSTED FOR BEST FIT

10:1 OR FLATTER   20 FT. MIN.   10:1 OR FLATTER

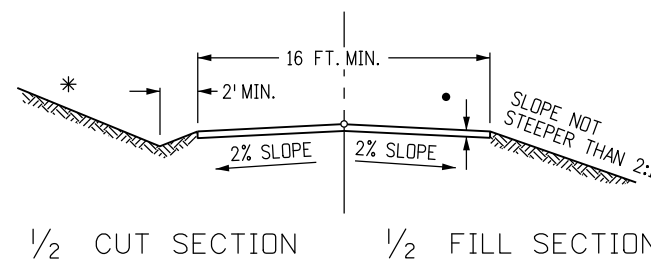## TYPICAL SECTION FOR MEDIAN CROSS OVER

ANY REQUIRED PIPE OR INLET FOR MEDIAN DRAINAGE SHALL HAVE A TRAVERSABLE DESIGN AS SPECIFIED ON THE PLANS

6:1 6:1

6:1 6:1

6:1 6:1

USE 6:1 OR FLATTER SIDE SLOPE IN THE APPROACH RADII AREA. THE SIDE SLOPES IN THE MAIN ROADWAY AND THE APPROACH ROADWAY, IF STEEPER THAN 6:1, SHALL BE SMOOTHLY TRANSITIONED INTO THE 6:1 AREA.

SHOULDER

EDGE OF TRAVELED WAY

SHOULDER

6:1 6:1

25 FT. MINIMUM RADII SHALL BE USED ON FIELD AND RESIDENTIAL DRIVES.

▲ WIDTH TO BE SHOWN ON PLANS
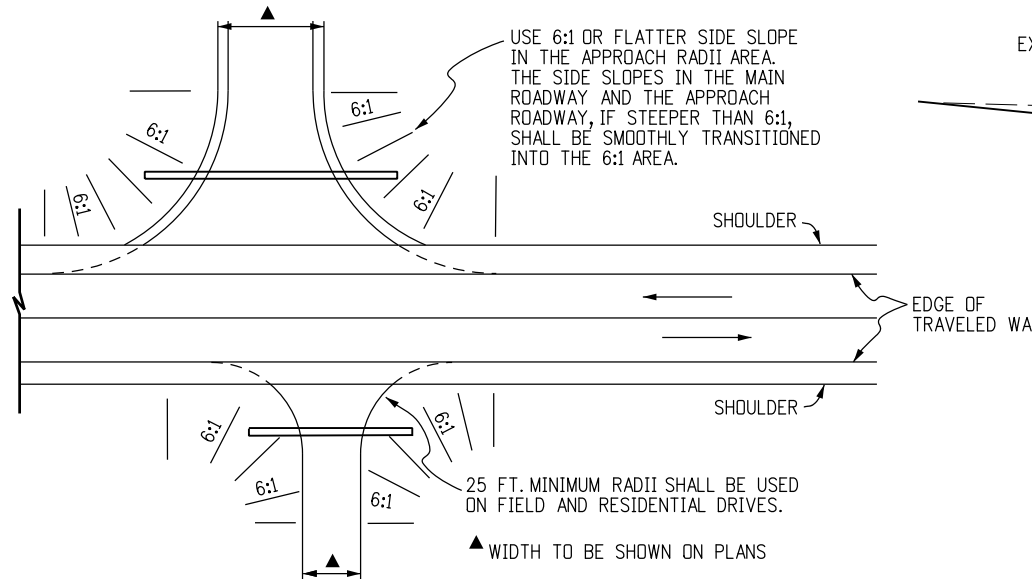
SIDE DRAINS SHALL BE LOCATED BEYOND THE CLEAR ZONE, OR WHEN WITHIN THE CLEAR ZONE, THEY SHALL BE INSTALLED WITH END SECTIONS CONFORMING TO A 6:1 SLOPE. FIFTY FT. RADII SHALL BE USED ON INTERSECTING ROADS, EXCEPT FOR FIELD AND RESIDENTIAL DRIVES OR UNLESS OTHERWISE SPECIFIED ON PLANS. RADII MAY BE VARIED TO SUIT FIELD CONDITIONS.
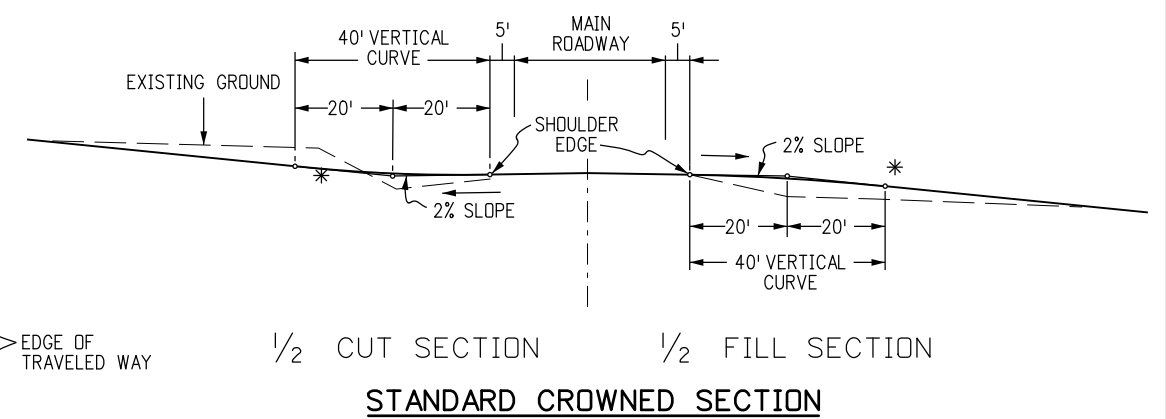
TYPICAL PLANS FOR SIDE APPROACH ROAD

---

16 FT. MIN.

2' MIN.

SLOPE NOT STEEPER THAN 2:1
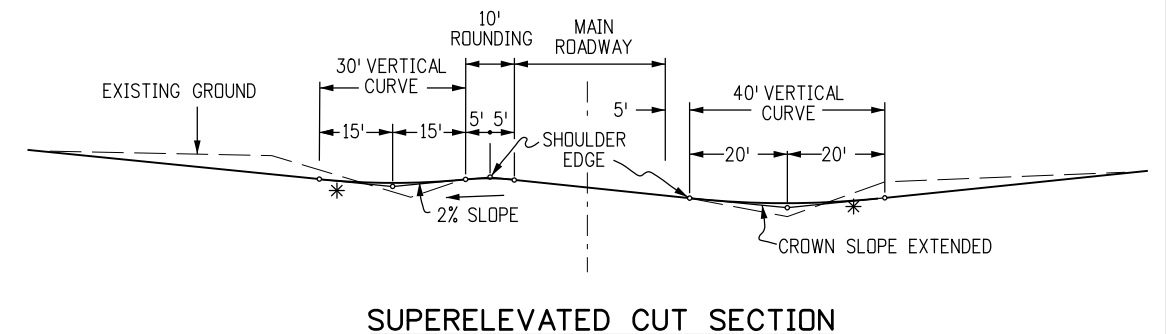
2% SLOPE   2% SLOPE

½ CUT SECTION   ½ FILL SECTION

TYPICAL SECTION FOR APPROACH (ACCESS) ROAD

NOTE: ROAD APPROACHES WHICH REQUIRE HMA (ASPHALT) PAVEMENT SHALL BE PLACED AT THE FOLLOWING DISTANCES BACK FROM THE ROADWAY EDGE OF PAVEMENT:
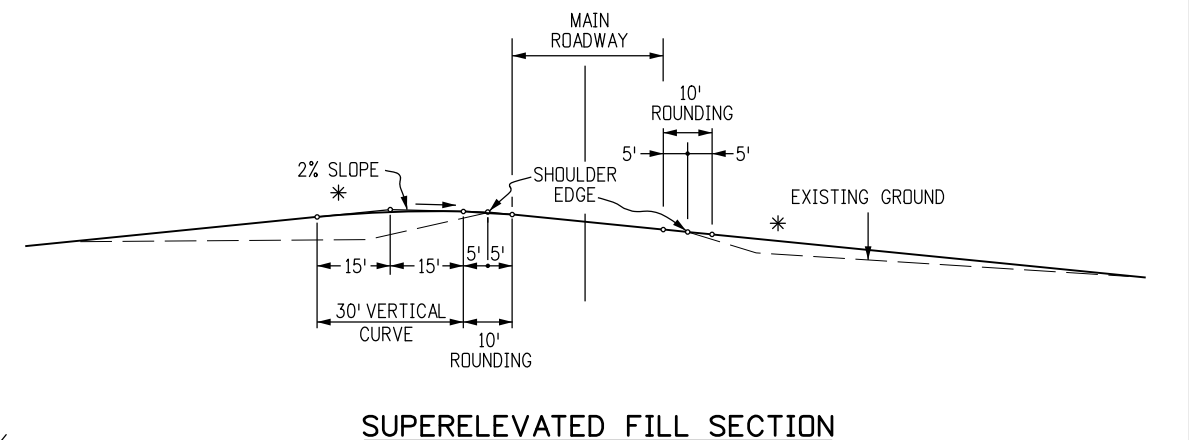
1. RESIDENTIAL OR AGRICULTURAL FIELD ENTRANCES - PAVE 4 FEET BACK.

2. THREE OR MORE RESIDENCES OR COMMERCIAL PROPERTY - PAVE 20 FEET BACK OR TO ROW LINE, WHICHEVER IS LESS.

3. PUBLIC STREET - PAVE 50 FEET BACK OR TO ROW LINE, WHICHEVER IS LESS.

4. IF EXISTING ACCESS IS PAVED, THEN FEATHER NEW ASPHALT OVERLAY A MINIMUM OF 2 FEET BACK OR AS DIRECTED BY THE ENGINEER.

EXISTING GROUND

40' VERTICAL CURVE

20' 20'

5' MAIN ROADWAY 5'

SHOULDER EDGE

2% SLOPE

2% SLOPE

20' 20'

40' VERTICAL CURVE

½ CUT SECTION   ½ FILL SECTION

STANDARD CROWNED SECTION

10' ROUNDING

MAIN ROADWAY

30' VERTICAL CURVE

15' 15'

5' 5'

40' VERTICAL CURVE

EXISTING GROUND

SHOULDER EDGE

5'

20' 20'

2% SLOPE

CROWN SLOPE EXTENDED

SUPERELEVATED CUT SECTION

MAIN ROADWAY

10' ROUNDING

2% SLOPE

SHOULDER EDGE

5' 5'

EXISTING GROUND

15' 15'

5' 5'

30' VERTICAL CURVE

10' ROUNDING

SUPERELEVATED FILL SECTION

VERTICAL ALIGNMENT SIDE APPROACH ROADS INTERSECTING MAIN ROADWAY

* TANGENT SLOPE NOT STEEPER THAN 8% BEYOND THE VERTICAL CURVE. THE SLOPE MAY BE STEEPER, IF REQUIRED, TO MEET EXISTING APPROACH SLOPE. HOWEVER, APPROACH ROAD SLOPE SHOULD NOT BE STEEPER THAN EXISTING SLOPE.